

GENERAL DESIGN GUIDELINES
AND DEVELOPMENT STANDARDS

3.0

3.0 GENERAL DESIGN GUIDELINES AND DEVELOPMENT STANDARDS

These General Design Guidelines and Development Standards address the architectural, landscape architectural and site development requirements that will create a quality environment for the Gateway Specific Plan Project Area.

The desired design character for the Project Area is western/early Californian. The City of Norco and its residents value the life style and atmosphere this theme represents. The City seeks to preserve the special qualities the life style is associated with; "City living in a rural atmosphere".

All new development in the Project Area must comply with the overall development guidelines contained herein and the Development Regulations (Section 4.0) to receive project development approval. These Guidelines will be used by the City as a basis for review of all proposed projects. Proposals that do not address these guidelines will not receive City approval.

There are key design elements which will contribute significantly to the overall visual impact of the Project Area. These include landscape design, architecture, and site planning criteria. This section addresses each of these elements in general terms and establishes the guidelines which will be used and expanded upon in more detail through the site planning and project approval process.

3.1 GENERAL GUIDELINES

This section is not intended to limit the work of designers, but to provide a flexible framework to accomplish a comprehensive design concept and to encourage quality development which will establish the Gateway Project Area as a special place to visit, work and/or own a business.

Design Goals

- o To create quality commercial, office park and industrial districts.
- o To create an identity for the Gateway Specific Plan Project Area specifically and the City of Norco in general.
- o To enhance the economic potential of the Project Area by attracting quality developments.

Design Guidelines

General qualities and design elements for buildings that are most actively encouraged within the Project Area are:

- o Richness of surface and texture
- o Play of light (shapes and shadows)

- o Equal void to solid building wall ratios
- o Multi-planned roofs and walls
- o High degree of varying wall articulation

The elements to avoid or minimize:

- o Large, extremely deep front setbacks, except where deeper setbacks are required, with vast areas of parking located there
- o Highly reflective surfaces
- o Large blank walls
- o Large metal surfaces
- o Exposed concrete block
- o Chain link fence, barbed wire
- o Inarticulated building facades

3.1.1 Business District Sign

Business district sign will be provided along Yuma Drive near the I-15 on ramp (see Exhibit 23). This sign, similar to one shown in Exhibit 23A, is designed to announce the entrance into a specific area of the City. The business district sign will feature materials that reflect a western/early California look, such as wood and river-wash cobble.

3.1.2 Directory Sign

Directory signs will be provided at the following locations (see Exhibit 23):

- A. Northwest and southeast corners of First Street and Hamner Avenue; and
- B. Northeast corner of Yuma Drive and Hamner Avenue.

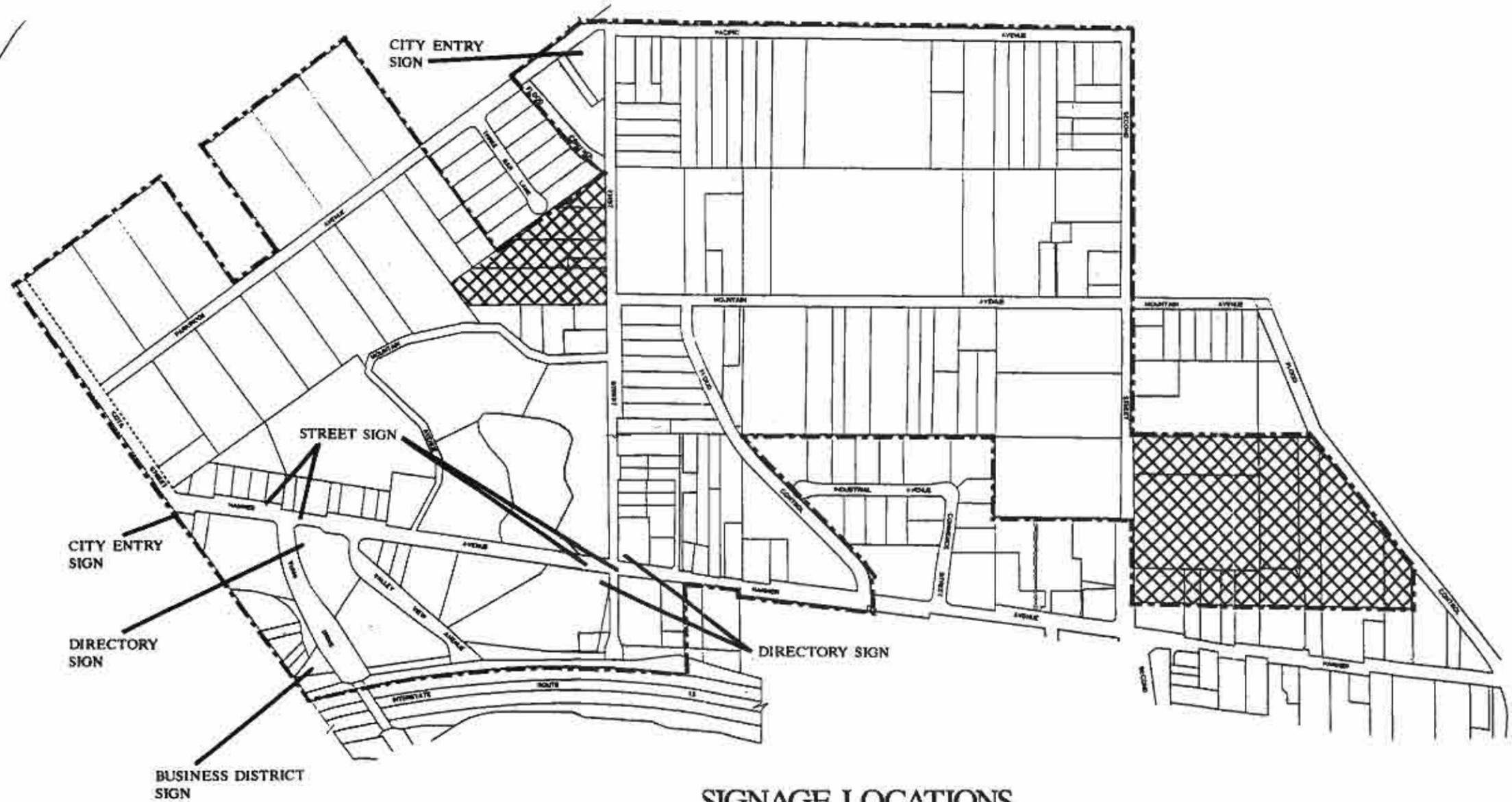
These signs, similar to the ones shown in Exhibit 23B, are designed to identify commercial businesses at that location. Directory signs will feature materials that reflect a western/early California look, such as wood and river-wash cobble.

3.1.3 Street Sign (median island sign)

Street signs will be provided at the following locations (see Exhibit 23):

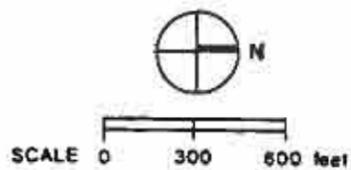
- A. Along Hamner Avenue at Yuma Drive; and
- B. Along Hamner Avenue at Second Street.

Gateway Specific Plan



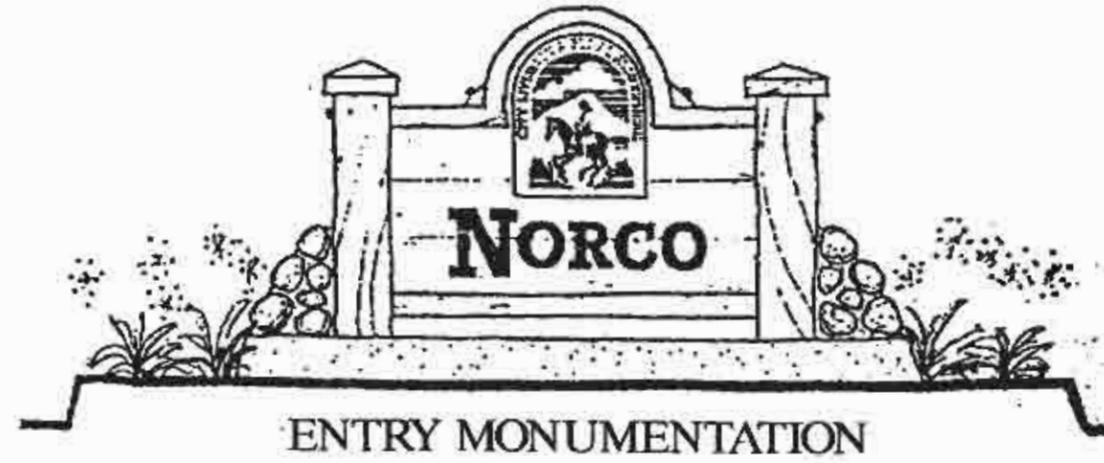
SIGNAGE LOCATIONS

XXXXX Not A Part

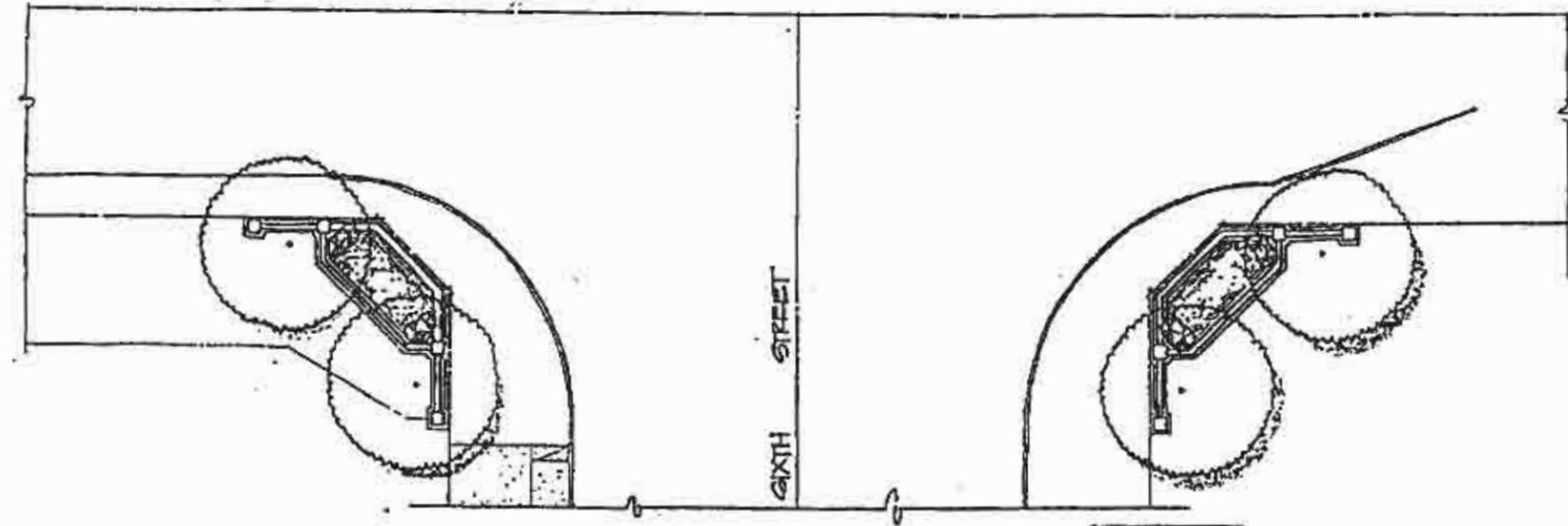


WHY DOES THIS MAP NOT INDICATE PACIFIC AS A CUL-DE-SAC?

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ENTRY MONUMENTATION



SIGNAGE SITE PLAN



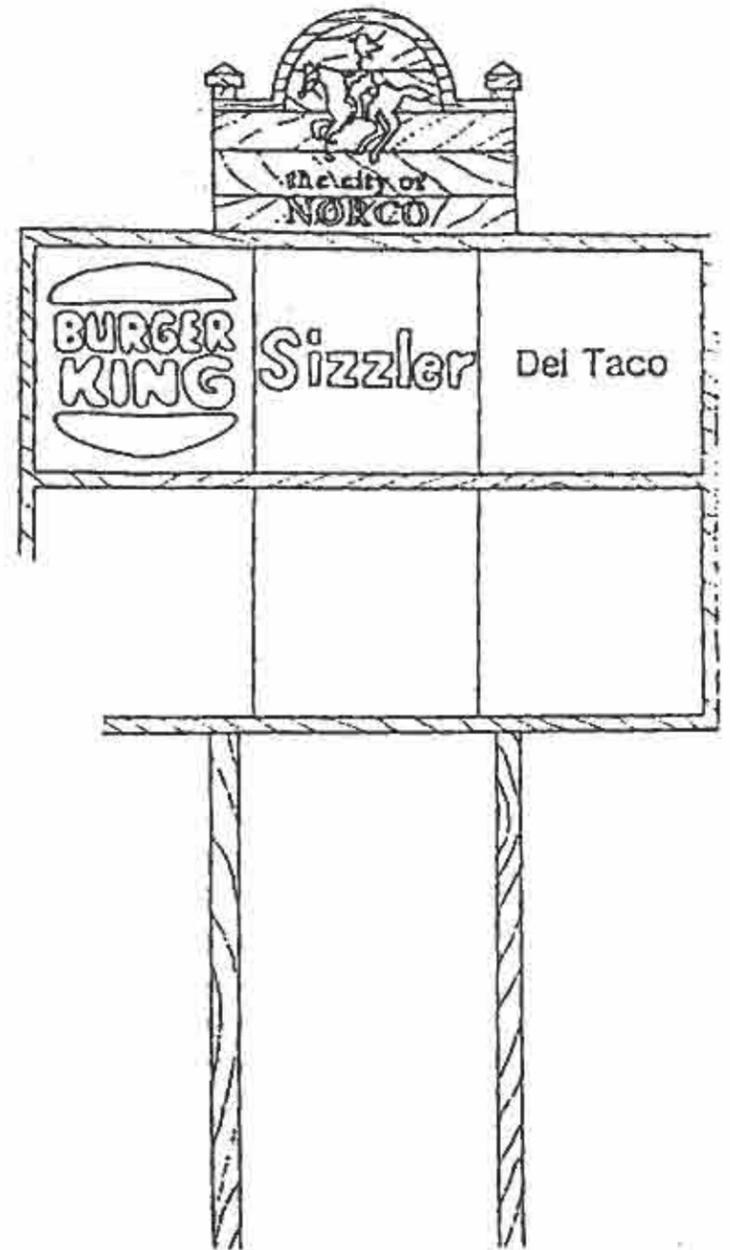
BUSINESS DISTRICT SIGN

ELEVATION
SCALE: 1" = 1'-0"

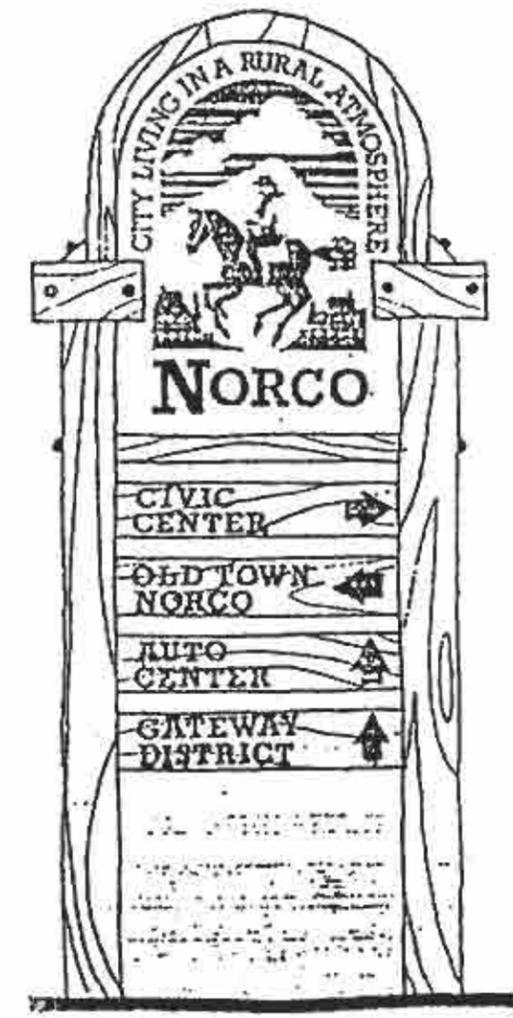
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DIRECTORY SIGN



DIRECTORY SIGN



DIRECTIONAL/
LOCATIONAL SIGN

These signs, as shown in Exhibit 23B, are designed to show directions to other significant locations (Civic Center, Old Town Norco, etc.) within the City. Street signs will feature materials that reflect a western/early California look, such as wood and river-wash cobble.

3.1.4 Entry Monumentation

Entrance monumentation will be provided at the following locations:

- a. Hamner Avenue and Cota Street; and
- b. First Street and Pacific Avenue.

These monuments, as shown in Exhibit 23, are designed to announce the entrance into the City and will promote a clear sense of "progression" and transition into the Project Area and the City. Entry signs will feature materials that reflect a western/early California look, such as wood and river-wash cobble.

3.1.5 Site Planning

Project Area site planning standards are as follows:

- a. In situations where buildings are highly visible from side streets and/or adjacent parking areas, blank walls shall be prohibited and special emphasis placed on creating "architectural interest".
- b. In the case of multi-storied buildings, it is important that multi-storied buildings relate to the pedestrian scale. All ground story facades in these areas shall be designed to relate to a pedestrian scale as previously shown in Exhibits 12, 13 and 14. This can be accomplished through breaking the facade into bays and the signage brought down in size and location. Pedestrian scale shall also be established through the use of pedestrian canopies and awnings which add horizontal articulation to facades. Landscape elements and planting areas shall also be used to strengthen the pedestrian scale and to encourage pedestrian activities in the commercial and office park districts.
- c. Within the Commercial District, buildings shall be designed to be visually connected in order to eliminate a strip commercial appearance.
- d. Architecture and outdoor space within the commercial and office park districts shall be integrally designed and oriented toward the pedestrian experience. The experience should be visually diverse, stimulating, and include activities and a built environment that creates a sense of variety and excitement.
- e. Create plazas and open space within individual development projects to the greatest degree possible.

- f. Site and design buildings to minimize pedestrian/vehicle conflicts.
- g. Site and design structures to facilitate public access across sites where important pedestrian connections occur.
- h. Orient/screen all auto related facilities (i.e., working bays, storage, etc.) from public view.
- i. Bi-level landscape buffers shall be provided between existing residential land uses and proposed commercial office park and industrial land uses. As shown in Exhibit 24, the first buffering "level" shall consist of a seven to eight foot high masonry wall. The second "level" consists of a landscaped buffer.
- j. Avoid long linear vistas and building edges within the development envelope and along the streetscape through variations in setbacks.
- k. Public entrances and siting of structures should address major streets. Lots located adjacent to Pacific Avenue are required to take primary vehicular access from the new street paralleling Pacific and Mountain Avenues. Pacific Avenue is to be used only for emergency vehicles.
- l. Primary elements of effective site design are illustrated below:
 - a. controlled site access
 - b. separate service area and service access
 - c. separate employee parking
 - d. convenient public access and visitor parking
 - e. front yard landscaping
 - f. screening of storage and work areas
- m. Within the Gateway Specific Plan Project Area it is more acceptable to create a clustering of smaller building forms connected by open space and walkways than to create a massive single building form.

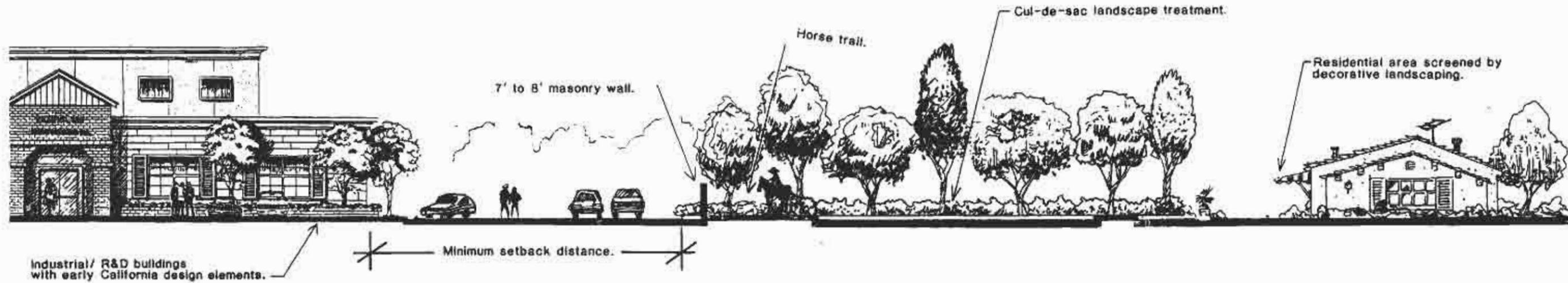
3.1.6 Architecture

Commercial and Office Park Districts

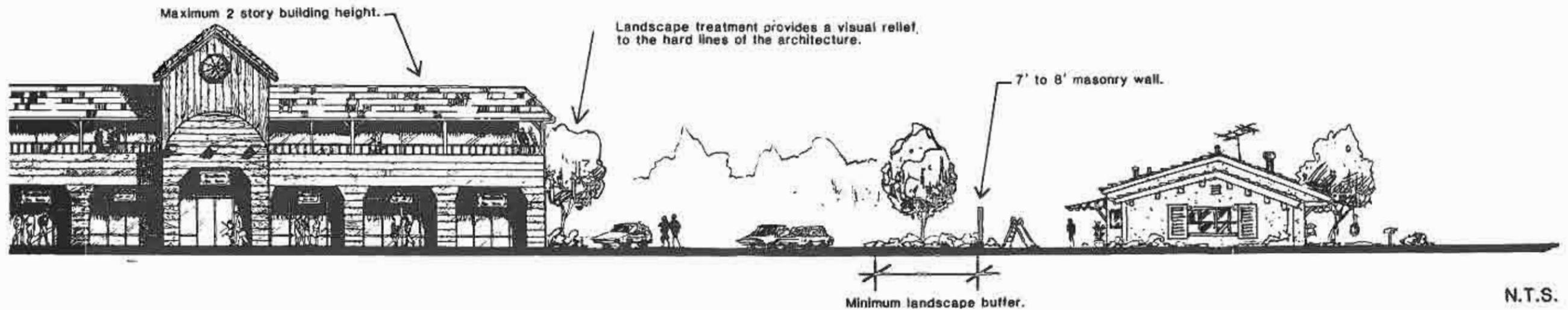
Generally, the design concept within the Commercial and Office Park Districts will emphasize building clusters with vehicular parking lots located away from pedestrian travel routes.

Buildings within these two districts should be diverse with a scale that is not overbearing relative to pedestrians; architectural style should retain a rural flavor consistent with the goal of Norco.

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PACIFIC AVENUE ROADWAY SECTION AND LANDSCAPE BUFFER DETAIL



TYPICAL COMMERCIAL LAND USE ADJACENT TO RESIDENTIAL LAND USE

The following architectural elements should be addressed when designing structures within the Commercial and Office Park Districts:

1. Roof Features
2. Clustering
3. Visual Connections
4. Fenestration (windows/doors and misc. openings)
5. Construction Materials
6. Color

Desirable application of the featured design elements are as follows (see Exhibit 25):

1. Roof Features/Visual Connections

- a. Buildings should have predominant roof features with varying roof lines created by: tower features, canopies, awnings, covered porches, exposed wood beams, and wood shingle or simulated wood shingle roof surfaces.
- b. Full roofs (rather than built-up roofs with facades) are encouraged for single-level structures, thus providing substantial roof mass.
- c. Extensive roof overhangs and extension of roof features (trellises) to accessory structures are encouraged.
- d. All elements on rooftops shall be attractively screened from public view. Mechanical equipment and duct work shall be depressed adequately within the roof structure or incorporated within special rooftop features. No equipment or duct work shall be allowed on the roof of any structure within view from any street, freeway or adjacent buildings. The screening shall be an integral part of the building (e.g., extended parapets), and shall be painted the same color as the rest of the building. Corrugated metal screening is not permitted.

2. Clustering

- a. The bulk of the building should be visually broken into at least two or three distinct masses.
- b. The clustering of groups of buildings connected visually by open space, covered walkways and trellises is encouraged.

3. Fenestration (windows/doors and misc. openings)

- a. Window treatments, door openings and exterior hallways, and detail should reflect the western/early Californian style.

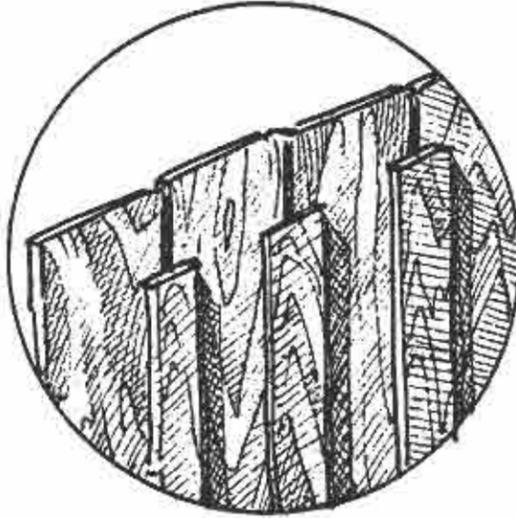
Gateway Specific Plan



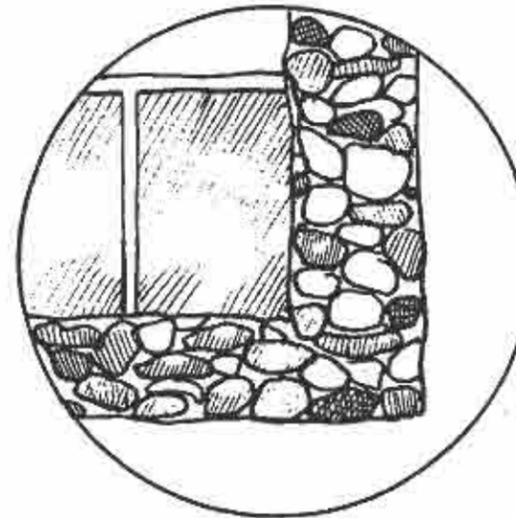
TOWER ROOFTOP FEATURE



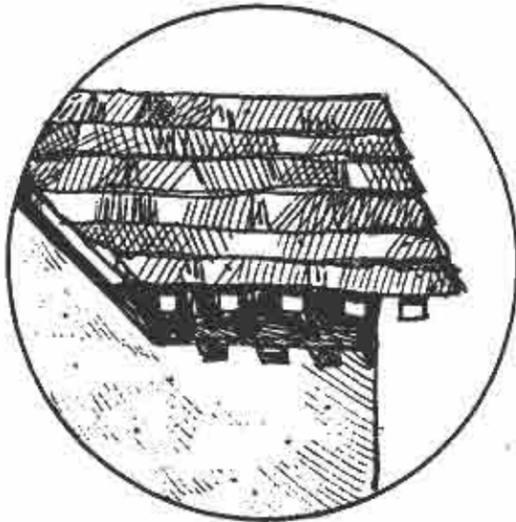
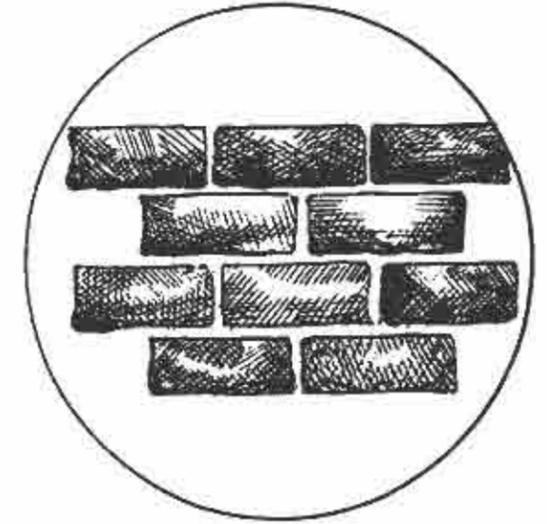
BOARD AND BATTEN



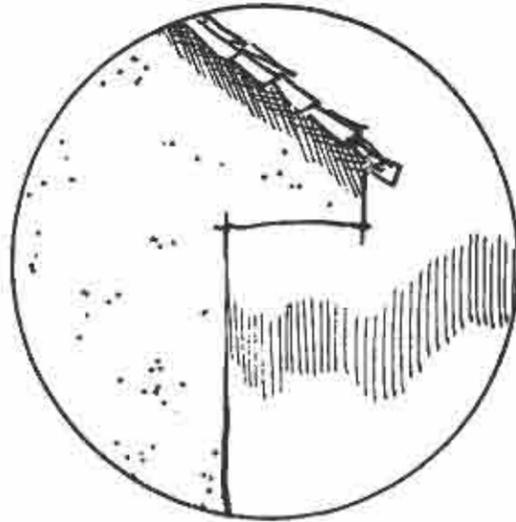
RIVER ROCK



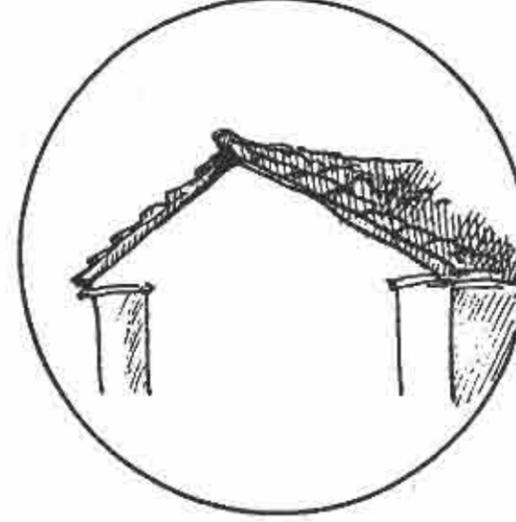
MASONRY



SHINGLES & RAFTER TAILS



TILE & STUCCO WALLS



HIP ROOF/GABLE ROOF



BARN SHED ROOF
& ROOF OVERHANGS

- b. Entry ways should be recessed or projected outwards.
- c. Use of predominant roof lines such as tower features should be focused around entry areas.

4. Construction Materials

The following general building materials alternatives are recommended to achieve a variation upon the desired architectural theme:

Wall Materials

- a. Exterior walls of all buildings are anticipated to be "off-white" or "tertiary earth tones of brown hues" and be constructed of Spanish style textured materials such as slump stone concrete block, Spanish style plaster, or adobe brick. The use of heavy appearing exposed wood beams is encouraged.
- b. Use of wood, brick and river rock exterior walls, wall and support elements are encouraged as an alternative to or as an adjunct to the Spanish style textured materials. Use of the "off-white" or "tertiary earth tones of brown hues" can be used to compliment and/or contrast with natural colors of unpainted wood, brick and rock work. Use of plant materials to compliment and weaken harsh vertical and horizontal lines are encouraged.

5. Roof Materials

- a. When visible, roofs shall be constructed of clay tile, wood shingle, wood shakes of appropriate color and texture of asphalt shingle. Wood shake and wood shingles may be used only by approval of the City Fire Chief.
- b. Where roof lines are not visible due to use of parapet walls, other types of roofing materials may be used pursuant to the requirements of applicable building and construction codes and regulations.

6. Color

- a. Colors for the exterior of the buildings must serve to integrate all elements of the commercial and office park districts and enhance the established style and character. The use of strong, earth tone colors which highlight the building structure is encouraged. Use of colors with a deep hue are encouraged in signage.

Industrial District

Buildings

Conventional industrial architecture can be characterized by simple one and two story rectangular box-type structures. Typical construction materials and/or veneers might include exposed aggregate concrete, river rock, brick/brick veneer and other masonry construction materials. The buildings can be trimmed with wood awnings, arbors, etc., to help create a more rustic quality within the Industrial District. These relatively low profile, simple shapes can comprise a strong element of continuity throughout the industrial district; fenestration details which will provide the greatest degree of variety should be employed. The following design elements should be addressed when designing buildings and structures within the Industrial District:

1. wall surfaces (texture and relief)
2. fenestration (windows, entrances)
3. colors
4. graphics
5. contrast of building materials
6. lighting

The varying and different applications of a variety of elements is only limited by the imagination and ingenuity of the architect. Applications of the described design elements are as follows:

1. Wall Surfaces

- o brick/brick veneer
- o exposed aggregate
- o scored split face block
- o river rock
- o wood veneer
- o opaque glass (no shiny reflective glazing is permitted)

2. Fenestration

- o recessed or projected entries
- o entry overhead elements
- o windows
- o clerestories
- o landscape planters
- o undulation of building massing

3. Colors

- o subtle warm tones

- o white
- o earth tone color bands and trim

4. Graphics

- o building address
- o directional
- o company names

5. Contrast

- o smooth to rough
- o scale
- o color
- o positive/negative space

6. Lighting

- o contrast
- o positive/negative
- o highlighting effects
- o directional

The following guidelines should be consulted when designing and reviewing future development anywhere within the Gateway Specific Plan Project Area:

1. Avoid long, inarticulated building facades. Buildings with varietal front setbacks are strongly encouraged.
2. Avoid blank front and side walls on street frontages.
3. Flat roofs with parapet walls to screen rooftop equipment are appropriate, although buildings with articulated varying roof planes are encouraged.
4. The use of prefab, all metal steel for sheathing of buildings is prohibited. This is not to preclude the use of metal detail within architecturally designed buildings such as "Cortin" steel.
5. Conceal all service areas and storage areas either within the building themselves or by screening walls by a solid masonry, concrete or stucco stud wall of one color.
6. Ultra flat smooth facades, and polished reflective surfaces are not encouraged within the Project Area.

7. All screening shall be architecturally integrated with the building design and a roof parapet wall shall be used to screen roof mounted equipment.

The following architectural elements shall be prohibited within the Project Area:

- o Highly reflective surfaces (steel)
- o Large blank walls
- o Flat roofs except where extended parapet walls are used to achieve a "western" character
- o Reflective glass
- o Metal siding
- o Plastic or aluminum siding; and
- o Irregular, modernistic window shapes and rhythms.

3.1.7 Parking

Project Area parking standards are as follows:

- a. Parking access points, whether located on Hamner Avenue or other streets, should be located as far as possible from street intersections. Specific access points as proposed for individual site plans must be submitted for review and approval by the Director of Public Works/City Engineer.
- b. Design parking areas so that pedestrians walk parallel to moving cars. Minimize the need for the pedestrians to cross parking aisles.
- c. Driveways and parking areas should be separated from adjacent sidewalks or landscaped areas by a curb not less than six inches high.
- d. All parking spaces shall be clearly outlined on the surface of the parking facility.
- e. Link individual project parking areas with on-site driveways which are clearly identified and easily recognized as connectors.
- f. Use parking lot paving surfaces such as turf block or gravel that will allow water to percolate down wherever practical and desirable.
- g. As a condition of project approval, the applicant may be required to combine parking facilities and access to serve more than one individual project.

- h. Required parking or loading facilities shall not be located in any required setback areas unless specifically allowed by the development regulations.
- i. Loading areas and docks shall be permitted only in rear and side lot areas and shall be screened from public view.
- j. No part of a street shall be used for loading.
- k. Separate automobiles access and parking from service truck areas.

3.1.8 Landscape Architecture

Project Area landscape design standards are as follows:

- a. Establish a colorful landscape edge at the base of buildings and avoid asphalt edges at the base of structures as much as possible. Plant materials located in containers are appropriate.
- b. Landscaping should result in a low profile image, i.e., use of blade grass lawn areas and other ground covers, canopy trees in parking areas, use of hedges, and low walls and plantings to screen service areas.
- c. On-site plant masses should assume a non-uniform arrangement. The diversity of massing types should be great enough to provide interest, but kept to a level which evokes a relaxed, natural feeling.
- d. Water conserving irrigation systems and drought tolerant plants shall be used in all public and private landscaping whenever appropriate.
- e. Landscape materials should enhance the major architectural design elements through the coordinated use of shrub and leaf colors, tree forms, plant material masses, and lighting.
- f. Plant material and massing should be designed to compliment architectural elevations and roof lines through color, texture, density and form on both the vertical and horizontal planes.
- g. A combination of landscaping, fences and walls shall be used on the perimeter of properties to define property limits, separate use areas and provide on-site security.
- h. Landscaping, fences and walls, or a combination thereof: shall be used on the perimeter of properties to visually screen and/or physically enclose outdoor storage areas, loading docks and ramps, transformers, storage tank and other appurtenant items of negative visual quality.

- i. See Appendix B for the recommended Plant Selection Palette.

3.1.9 Riding Trails

The City of Norco has developed as a community which emphasizes equestrian activities. Existing City riding trail standards serve to compliment horseback riding while safeguarding the public through the provision of a regulated equestrian- pedestrian trail system.

Section 18.28 of the Norco Municipal Code establishes a zone within existing public easements and rights-of-way, and within private access easements that governs this trail system.

Within the Gateway Specific Plan Project Area, riding trails shall be located along the following roadways until such time that existing residential properities are recycled to commercial or industrial uses:

Second Street
First Street

Equestrian riding trail along Pacific Avenue shall remain a permanent installation.

3.1.10 Signage

Project Area signage standards are as follows:

- a. Signage shall be simple and express an easy to read, direct message. Brand names of merchandise shall not be displayed.
- b. Storefront shall not have more than two signs - one primary and one secondary. The secondary sign, shall be pedestrian in scale and not be visible by motorists.
- c. Signs shall identify only the firm name or major enterprise on the premises on which the sign is located. Material and product advertising shall not be permitted except as primary identification of an establishment. Additional identifying logos of products may be incorporated within the signing but are considered part of the overall allowed sign area.
- d. Pedestrian-oriented signs may be applied directly to the face of the building and may not exceed city sign area requirements. The shape of the sign can be a positive feature.
- e. No signs shall extend higher than the ridgeline or be located on the roof of the structure.
- f. Window signs shall not obscure more than 25 percent of the display window area.

- g. All signs, including temporary signs, shall be in conformance with the City of Norco Municipal Sign Code.
- h. All signs shall be for the purpose of identification or direction only and shall contain no advertising.
- i. Signs suspended from canopies, balconies, etc., will be permitted; adequate head room must be provided.
- j. There shall be no revolving, flashing, or moving signs.
- k. Wall-painted signs will be permitted with prior approval of the Director of Community Development. Such signs will compliment the western character of the Project Area.
- l. No pennants or similar devices for commercial display shall be permitted.
- m. Roof signs will not be permitted.
- n. Plastic and fluorescent light signage fixtures are not permitted, except for special events.
- o. All buildings using rear parking lots shall clearly identify their businesses with rear wall signage. When site planning requires rear parking within commercial and office park districts, rear entries shall not be treated as an "after thought" but shall reflect appropriate design considerations that attract pedestrians and encourage entry. Appropriate design features would include awnings, potted plants, pavement treatments, window placement, signage, street furniture and decorative painting around doors, windows, etc. Safe nighttime entry shall be facilitated through the use of appropriate lighting standards.

All structures shall exhibit clear address information on the front facade and back facade when rear entries are used. (Recommended guideline: minimum address number size shall be 6" for a 1 story building and 10" for a 2 story building.) For nighttime identification, the numbers shall be internally or externally lit. Otherwise all signing shall comply with the Norco Sign Ordinance.

3.2 STREETScape DESIGN

3.2.1 Center Median Islands

Center median islands shall be provided along Hamner Avenue.

These medians are designed to provide access control and a unifying landscape statement. As shown in Exhibit 26, the medians will incorporate rolling turf berms, low growing shrubs and

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Landsaped medians provide safety and roadway delineation in an eye pleasing fashion.

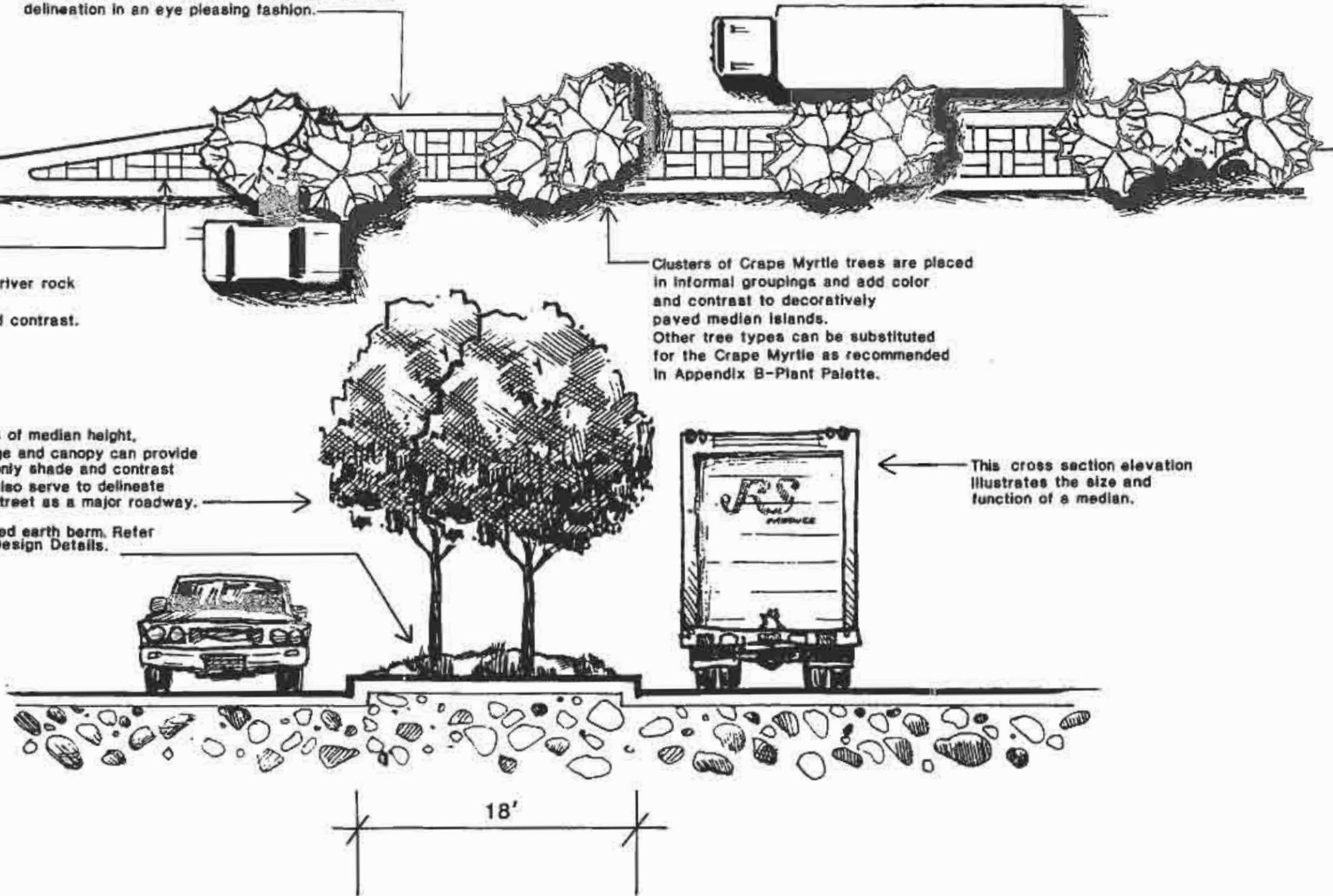
Median can be paved in stamped concrete as shown or natural materials such as river rock or other stone with areas of bermed landscaping to add contrast.

Clusters of Grape Myrtle trees are placed in informal groupings and add color and contrast to decoratively paved median islands. Other tree types can be substituted for the Grape Myrtle as recommended in Appendix B-Plant Palette.

Trees of median height, foliage and canopy can provide not only shade and contrast but also serve to delineate the street as a major roadway.

Suggested landscaped earth berm. Refer to Exhibit 15 for Design Details.

This cross section elevation illustrates the size and function of a median.



HAMNER AVENUE MEDIAN DESIGN
(CONCEPTUAL ONLY)

riverrock/cobblestone or stamped concrete, dominated by informal clusters of Crape Myrtle and Evergreen Pear or other trees as specified in Appendix B.

3.2.2 Streetscape Lighting

It is the intent of streetscape lighting to add a consistent, planned look to the Project Area during daylight hours and evening. Lighting can have a dramatic affect on the quality of streetscape design.

- a. All lighting fixtures in the public right-of-way shall be consistent and approved by the City Engineer following the recommendation of the Planning Commission. As shown in Exhibit 27, materials must be consistent and in keeping with the western/early Californian theme. Luminaries, poles and supporting hardware must also be consistent.
- b. Lights shall not be placed to cause glare or excessive light spillage on neighboring sites. A uniform light color is preferred for the area. The use of high pressure and energy efficient sodium lights is encouraged.
- c. Light standards shall not exceed 20 feet in height and in no case shall they exceed the height of the buildings on-site.
- d. The design of light fixtures and required structural support shall be architecturally compatible with the surrounding buildings.

3.2.3 Street Tree Program

All Project Area roadways shall incorporate a street tree planting program (see Exhibit 28); permitted street trees are those trees identified in Appendix B. Chapter 12.12 of the Norco Municipal Code provides tree installation, staking, trimming and maintenance requirements that will be applicable to the Project Area street tree program.

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Themed street light fixture mounted to a decorative wooden pole.



Height of pole conforms to minimum requirements provided by the City.

Natural wood sign with burned or applied decorative lettering.



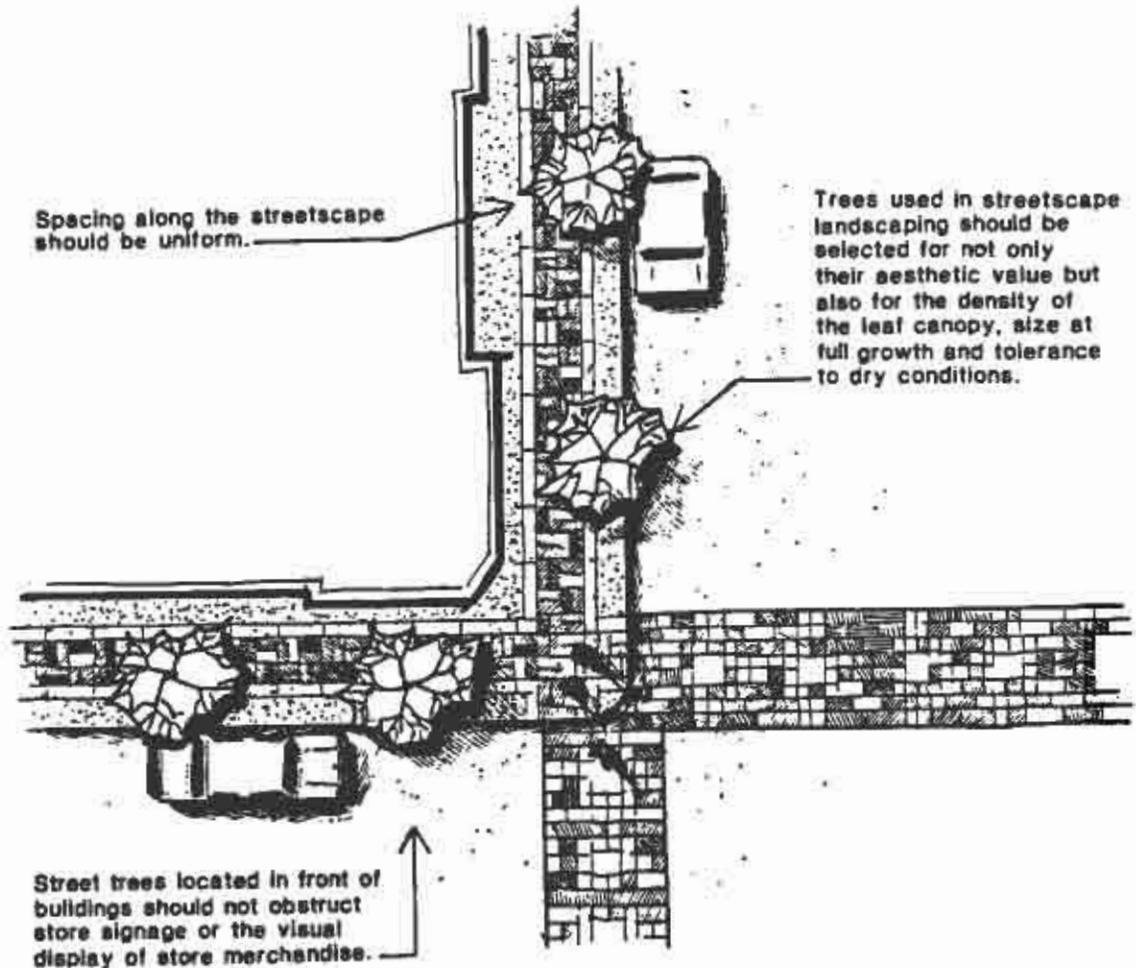
Directional/Locational street signage is suspended from street light fixture.

Decorative support base provides additional aesthetic elements to the streetscape materials such as natural stones and drought tolerant landscaping.



STREET TREE PROGRAM

Gateway Specific Plan



Spacing along the streetscape should be uniform.

Trees used in streetscape landscaping should be selected for not only their aesthetic value but also for the density of the leaf canopy, size at full growth and tolerance to dry conditions.

Street trees located in front of buildings should not obstruct store signage or the visual display of store merchandise.

STREETSCAPE LIGHTING DESIGN STANDARDS (CONCEPTUAL)